

Gravel Hill

Gravel Hill



Gravel Hill is the eastern most part of North West Cambridge, separated from phase 1 by the Girton Gap in the west, which also extends to the south and incorporates the Travellers Rest Pit SSSI. Gravel Hill is a mixed-use area with residential areas and student accommodation north of Storey’s Way and an academic quarter to the south.

Green and Blue Infrastructure

Character and levels

GH.01. Build from existing assets

Storey’s Wood and Cricket Pitch wood, Horse Chestnut Avenue, Category A, TPO trees and existing natural and designated features, including hedgerows, **must** be retained in accordance with the arboricultural survey and impact assessment recommendations;

To preserve and enhance the natural assets of the site for amenity, cultural and biodiversity value.

GH.02. Neighbourhood Greens

Design proposals **must** include a dedicated green open space including playable space and community gardens.

To support the site wide open space strategies.

GH.03. Managing level changes

Defensible space in front of buildings **must** allow sufficient space for stepped or ramped level change to / from the public realm to facilitate at least one level threshold per unit or communal entrance

To ensure inclusive access for all.

Community and amenity

GH.04. Informal open Space

Design proposals **must** provide informal open space.

To provide a site wide network of open space and amenity.

GH.05. Play

Gravel Hill **must** include playable space.

To support site wide play and amenity strategy.

▶ Refer to Sitewide / Green and Blue Infrastructure / Community and Amenity

GH.06. Community growing

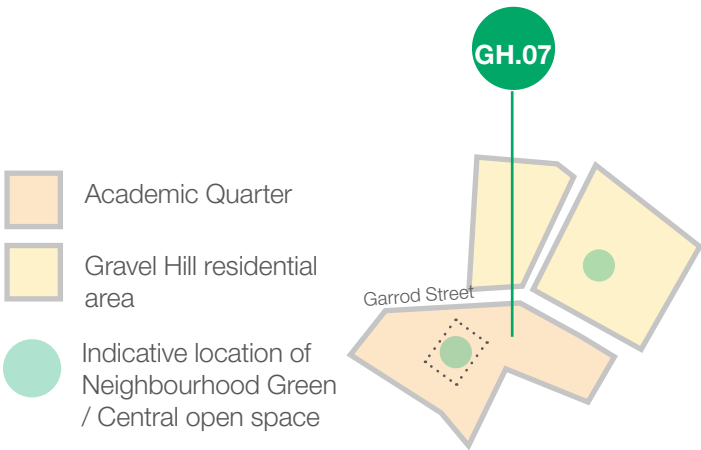
Community garden growing spaces **must** be provided close to a building and overlooked by homes. An external water point, seating and accessible routes must be included.

To support sustainability objectives providing space for food production.

GH.07. Academic Square

The design of the public realm **must** provide a sense of orientation and focal point. This should include a landmark tree and seating.

To create a legible public realm



Planting and biodiversity

GH.08. Existing ecological corridors

Existing ecological corridors such as Horse Chestnut Avenue **must** be retained in situ and enhanced where appropriate, following with ecological advice.

To support the site wide open space strategies.

GH.09. Girton Gap

Girton Gap **must** be retained as predominantly green open space which must maximise potential for habitat creation

To support biodiversity strategies across the site, and respect the character of the Girton Gap

GH.10. Trees on streets

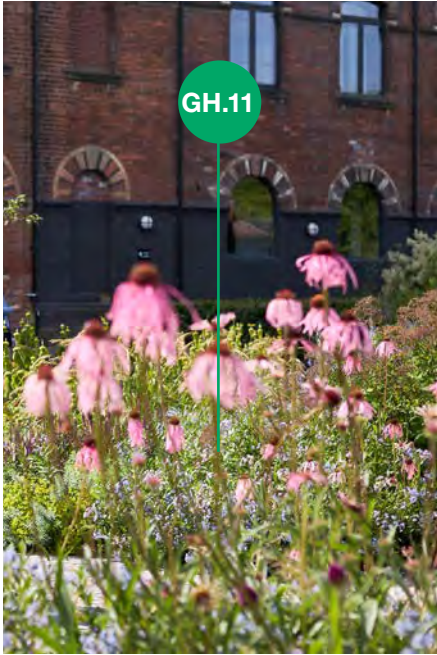
Trees on streets **should** be arranged in linear rows that contrast with informal groupings of other areas such as Brook Leys or Shared Gardens.

To create a distinctive sense of place and legible public realm

GH.11. Planting

Planting **must** be incorporated within the streets;

To enhance biodiversity, and maximize soft landscape in the public realm.



Water and drainage

GH.12. Permeable paving

The public realm **must** include permeable paving. This must be applied to car storage bays, and should be considered for all footpaths and pedestrian or cycle routes.

To support site wide surface water drainage strategy and masterplan sustainability vision.

GH.13. SuDS

SuDS beds **must** be incorporated into the public realm, and must support planting to enhance biodiversity. SuDS features could include rain gardens, bioretention beds or swales where space allows.

To support site wide surface water drainage strategy and masterplan sustainability vision.

▶ Refer to Sitewide / Green infrastructure / Water and drainage chapter.



Movement

Connection to existing routes

GH.14. Pedestrian and cycling connection to Gravel Hill

Design proposals **must** connect Gravel Hill with the Ridgeway. Pedestrian routes should have a minimum width of 2 metres. Cycling routes should have a minimum width of 3 metres.

To promote active travel.

Refer to Sitewide / Movement / Cycle strategy and parking

GH.16. Pedestrian and cycling access from the Local Centre

Design proposals **must** extend and connect to existing cycling and pedestrian routes leading to the Local Centre. These routes should be lit. Pedestrian routes should have a minimum width of 2 metres. Cycling routes should have a bi-directional width of 3 metres.

To promote active travel.

GH.15. Pedestrian and cycling access through Horse Chestnut Avenue

Design proposals **must** allow pedestrian and cycling access from Horse Chestnut Avenue extending into the Academic Quarter. External lighting types and locations must be designed and positioned to minimise impact on existing tree root protection areas. Existing type of lighting provision to Garrod Street should be maintained. These routes should be lit and a minimum of 3 metres wide.

To promote active travel.

GH.17. Vehicular access through Garrod Street

Design proposals **must** provide a main vehicular access through Garrod Street.

To consolidate vehicle movements.



Academic quarter - streetscape

GH.18. Connection to Madingley Rise

Connection from the site to Madingley Rise **must** be pedestrian and cycling priority with opportunity for vehicular access.

To create inclusive and equitable transport options, supporting active lifestyles.

GH.21. Active travel network

The movement network **must** prioritise cycling and walking.

To create inclusive and equitable transport options, supporting active lifestyles.

GH.19. Publicly accessible

Streets in the Academic Quarter **must** be publicly accessible.

To create inclusive public open spaces.

GH.22. Car storage

Car storage **must** be clustered in a way that reduces car presence on the street. For example on a corner or edge of the Academic Quarter. Car storage areas should include landscaping to soften clustered hardscape.

To allow streets to reduce the visibility and the connection to cars.

GH.20. Connected cycle lanes

The Academic Quarter **must** provide easy access to existing cycling routes.

To promote active travel.



Residential Streetscape

GH.23. Publicly accessible

Residential streets **must** be publicly accessible.

To create inclusive public open spaces.

GH.24. Pedestrian priority

Pavements **should** prioritise crossing points, for example through raised tables or other forms of pedestrian crossing prioritisation, in particular in corners or key crossings including connection to the Community Lane.

To ensure slow vehicular design speeds and convey pedestrian priority.

GH.25. Narrowing carriageway

Carriageway width **should** narrow down to the minimum required for each segment of road. For example, it could widen in front of bay car storage but narrow down where car storage is laid out in parallel.

To create intimate streets which facilitate social interaction.

GH.26. Slow-speed streets

Streets **should** include a change in geometry, materials and direction where practical.

To ensure slow vehicular design speeds and convey pedestrian priority.

► Refer to Sitewide / Movement / Vehicular access and car storage

GH.27. Car storage solutions

The residential areas **must** seek to reduce street car storage where practical. This could be achieved, for example with rear car storage, integral car storage or similar.

To allow streets to reduce the visibility and the connection to cars.

GH.29. Visitor cycle parking

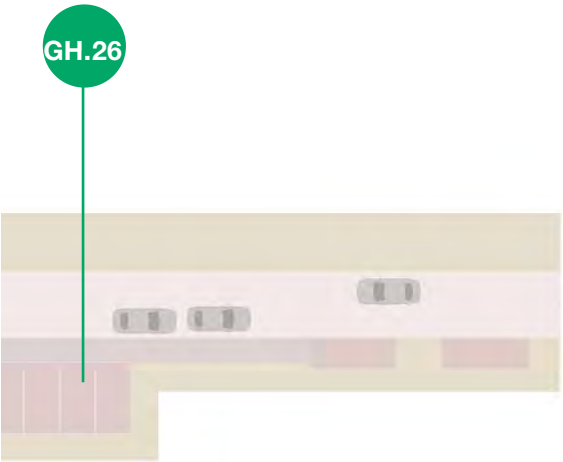
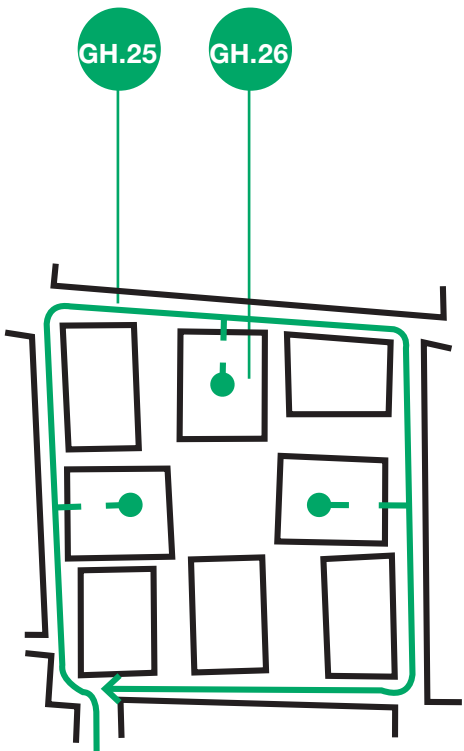
The Community Lane **must** include visitor cycle parking for a range of bike storage types. Visitor cycling storage should be located next to other street furniture, such as seating; to foster social interaction. Cycle parking should not be located so that it becomes an obstruction to access routes.

To create inclusive and equitable transport options, supporting active lifestyles.

GH.28. Cycling

Active travel **must** be prioritised, with access to active modes of travel provided. This could be through cycle storage provided by front doors.

To create inclusive and equitable transport options, supporting active lifestyles.



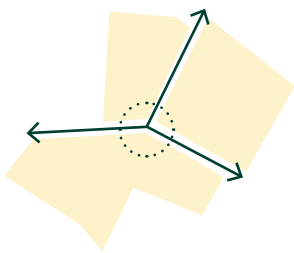
Built Form

Layout - general

GH.30. Mixed use hub

A mixed use hub **should** be located at the intersection of Garrod Street, Gravel Hill and Horse Chestnut Avenue, to create a focal space. This could be achieved, for example through creating an area of visual interest through vistas or architectural detail, and / or through clustering shared facilities or ancillary uses.

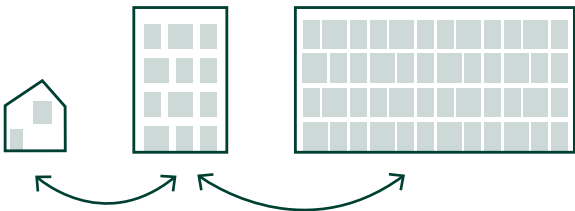
To create a focal point for social interaction.



GH.31. Architecture and character

Building design **must** contribute to a common character across building of different use classes. This could be achieved, for example through similar composition, tone and materiality, window type or other architectural detail.

To maintain a common family of buildings.



Academic Quarter

GH.32. Buildings defining a central open space

Buildings **should** be laid out to define a central open space. It should sit between different uses.

To provide a space that blends communities and fosters social interaction.

GH.33. Edge to Girton Gap

Buildings **must** be laid out to define the edge of the public realm facing the Girton Gap and their façades must positively contribute to it for example through facade hierarchy, openings, tone and materiality or architectural detail.

To provide a distinct edge to the development.

GH.34. Edge to Girton Gap - ground floors

The ground floor of the buildings facing the Girton Gap **must** make a positive contribution to this space. This could be achieved, for example by avoiding blank façades, servicing and services.

To activate streets and provide passive overlooking.

GH.35. Transition between built form and landscape

The design of the academic buildings **must** create transitions between built form and landscape. This could be achieved, for example through thresholds that are well landscaped.

To create a distinctive sense of place and legible public realm.

GH.36. Expression of shared amenity

Shared spaces and amenity **should** be expressed in the built form to promote social interaction and wayfinding. This can be achieved through a change of material or expression in facade.

To provide a space that blends communities and fosters social interaction.

GH.37. Student housing thresholds

Student housing threshold spaces **must** have a robust materials palette.

To facilitate frequent occupancy change over.



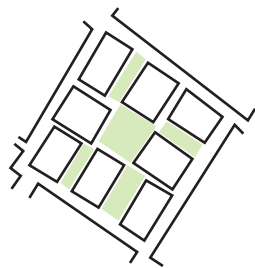
Residential

GH.38. Buildings shaping movement

Buildings **must** be laid out and aligned to form a loop movement system with one connection to Garrod Street.

To create healthy, low-speed neighbourhoods.

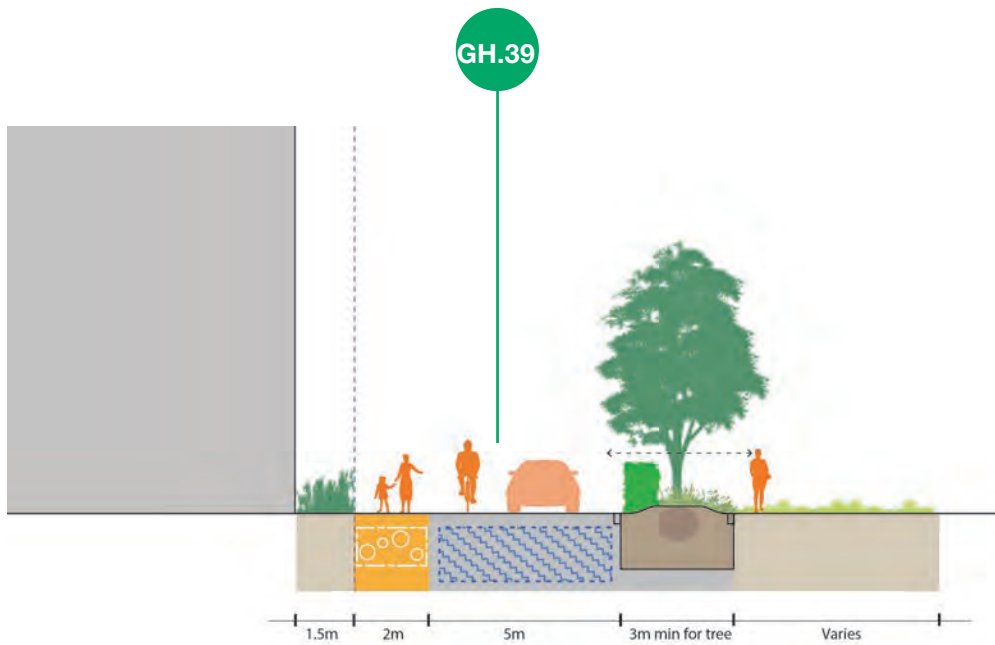
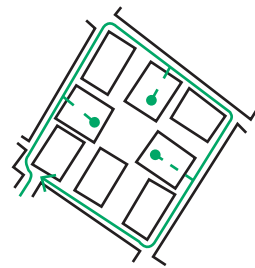
► Refer to Sitewide / Movement / Vehicular access and car storage



GH.39. Street widths

Residential street width **should** be as narrow as practical, please see illustrative section below for reference.

To create healthy, low-speed neighbourhoods.



GH.40. Private amenity and Horse Chestnut Avenue

Private amenity space and any associated boundaries **must** respect the root protection area of TPO trees within Horse Chestnut Avenue, and must employ appropriate methods of delivery and long-term management approved with the relevant Local Authority.

To create a robust and sustainable legacy.



Massing and character

GH.41. Variations in roofscape

Roofscape types **should** be distributed to create varied streetscape.

To contribute to the character of the Neighbourhood streets.

- Refer to Sitewide / Built Form / Massing and character.

GH.42. Tone and materiality

Tone and material selection **must** be selected to distinguish massing and avoid visual coalescence.

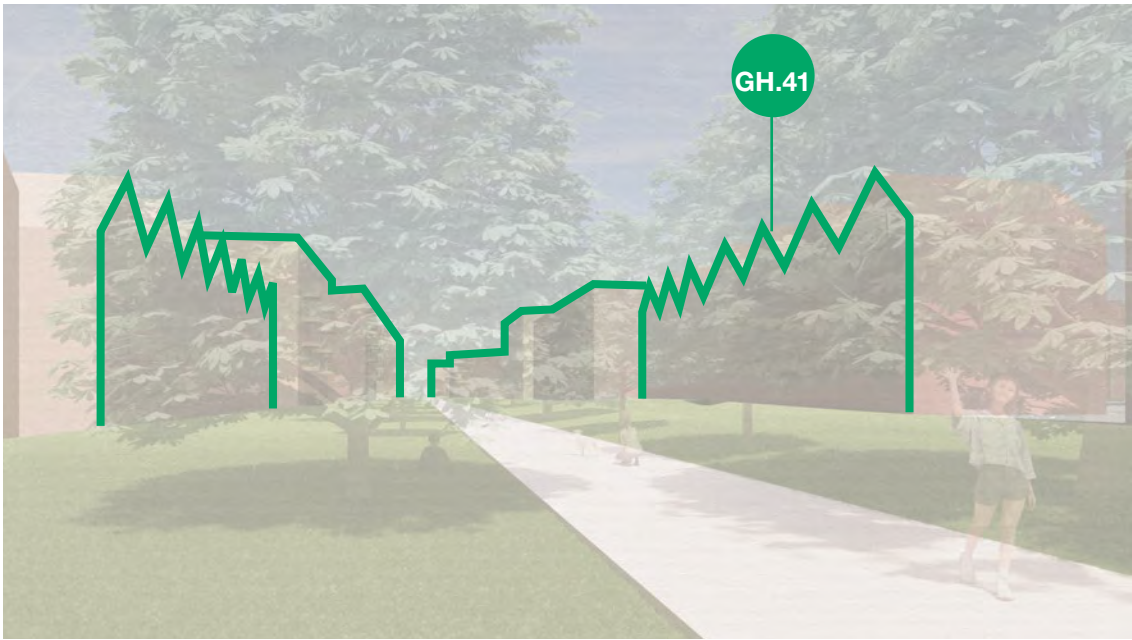
To contribute to the legibility of the massing.

- Refer to Sitewide / Built Form / Massing and character.

GH.43. Built form Horse Chestnut Avenue

Built form extents **must** not encroach on the canopy spread and root protection area of existing trees within Horse Chestnut Avenue.

To create a robust and sustainable legacy.



Cartwright Avenue & The Common

Cartwright Avenue & The Common



Cartwright Avenue is the key primary road through the Future Phases providing access to the new neighbourhood via cycle, foot and car. The Avenue will be a generous route lined with large growing trees.

Cyclists and pedestrians will be provided with generous paths and vehicle speeds will be controlled through the highways design. Cartwright Avenue opens out into the Common which is a larger public open space with tree planting providing spaces to sit.



Green and Blue Infrastructure

Character and levels

CA.44. Distinctiveness

The character of Cartwright Avenue and The Common **must** be distinctive from that of the Neighbourhoods. This could be achieved, for example, through materiality, pavement, tree species, and / or street furniture.

To create a clearly distinct movement spine for orientation and legibility in the public realm.

CA.45. Continuity

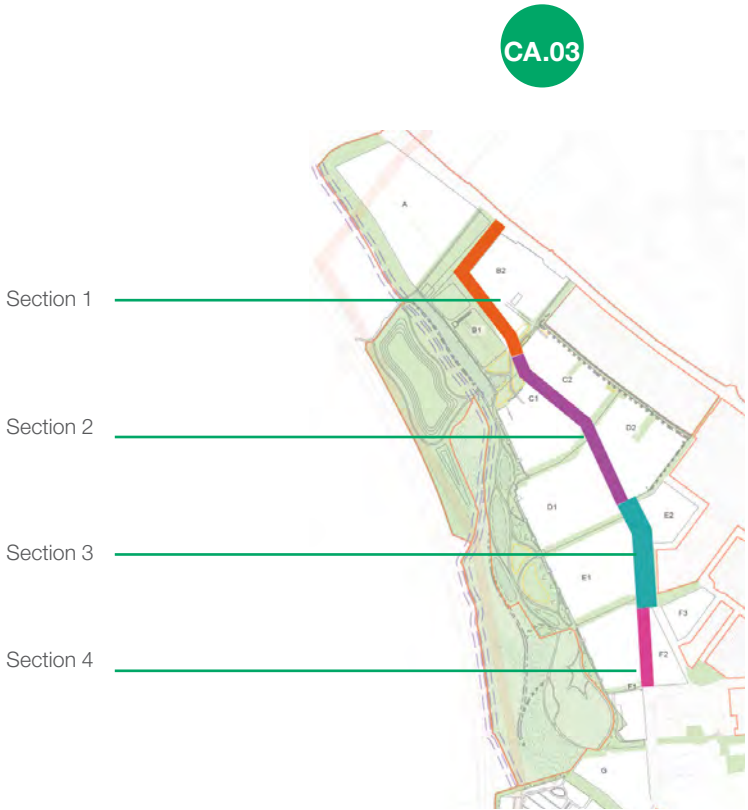
Cartwright Avenue **must** have continuity along its length. This can be achieved with a palette of paving materials being established and consistent streetscape character.

To create a common language, strengthening Cartwright Avenue as a clearly distinct movement spine.

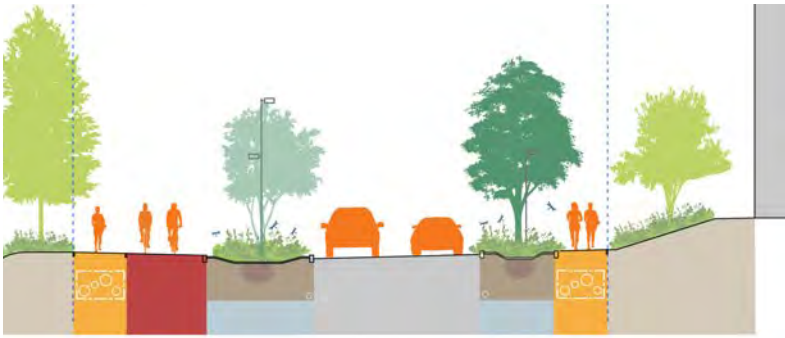
CA.46. Variety

The character of Cartwright Avenue **must** transition along its length defining different characters. These changes in character must reflect the specific conditions and uses of each area, for example, the location of mobility hubs and bus stops, the transition from unidirectional to bidirectional systems, layout and massing transitions at The Common or in front of the sports area.

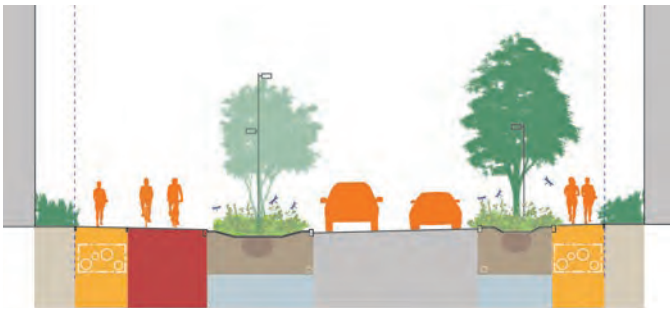
To create clearly distinct character areas for orientation and legibility in the public realm.



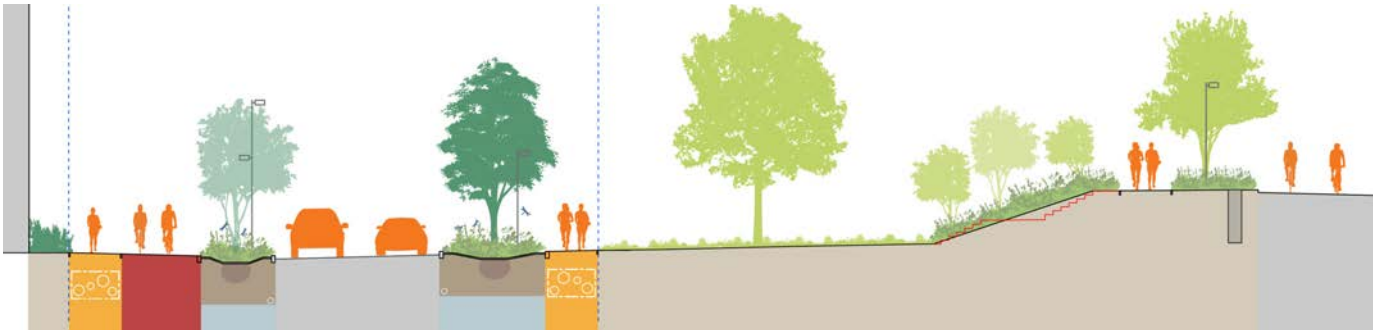
Illustrative sections



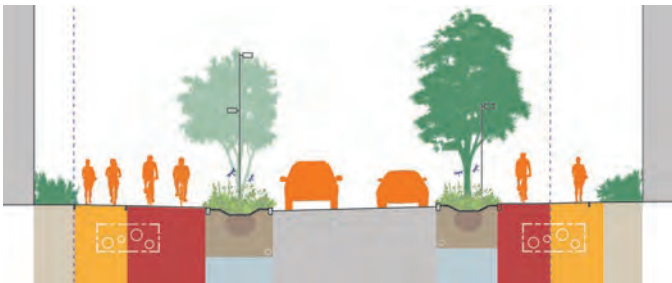
Section 1



Section 2



Section 3



Section 4

Community and amenity

CA.47. The Common

The Common **must** be designed to read as a singular space. Character definition could be achieved, for example through consistent materiality, pavement, tree species, and / or street furniture.

To reinforce sense of place.

CA.49. Steps and ramps in The Common

Where level changes must be addressed with steps & ramps, these must be integrated as part of the landscape design. Stepped and ramped routes should have equal importance and both routes be convenient and direct.

To ensure inclusive access in the public realm.

CA.51. Space to congregate at The Common

The Common **must** be designed as an area of congregation along Cartwright Avenue. This should be achieved, for example, through the location of seating, shaded areas, space for informal play or events.

To support the site wide active travel objectives and sustainability vision.

CA.53. Play

The Common **must** include playable spaces.

To support site wide play and amenity strategy.

▶ Refer to Sitewide / Green and Blue Infrastructure / Community and Amenity

CA.48. Level changes at The Common

The design of The Common **must** integrate the level difference between the eastern and western, for example through the inclusion of stands or steps or seating areas with good visibility across the public realm and through buildings.

To create legible and accessible public realm.

CA.50. Lighting

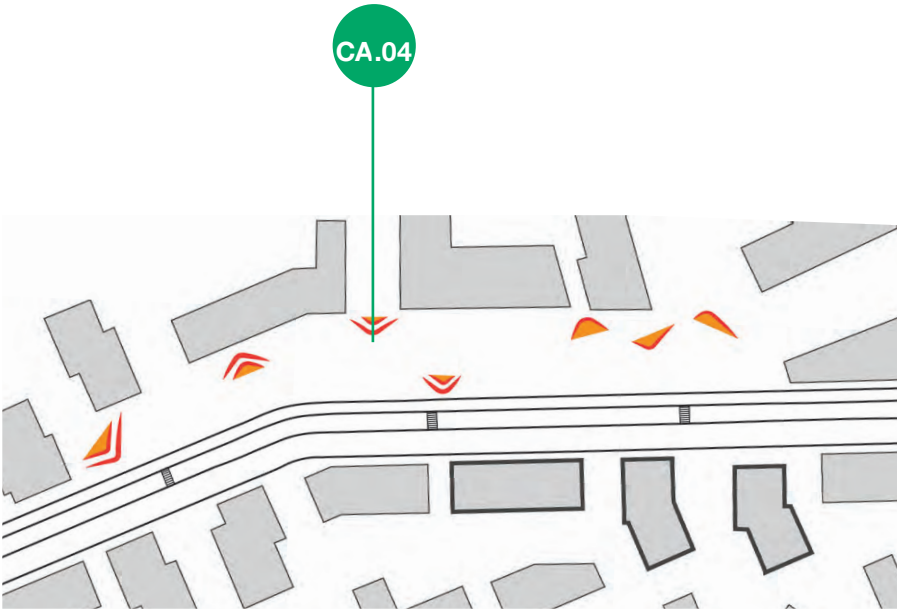
Lighting design approach **must** provide for a combined corridor catering to drivers, cyclists, and pedestrians.

To ensure efficiency and minimise visual clutter in the streetscape.

CA.52. Space for small informal events

The Common **must** include an area of hard standing with suitable space for ad-hoc pop-up events. This should include spaces for temporary vehicles, power connection and be integrated within the landscape proposals.

To provide high quality flexible use public realm space.



Planting and biodiversity

CA.54. Trees

Standard form trees **must** be included on both sides of the carriageway, with multi-stem trees integrated between standard form trees where visibility splays allow.

To create a consistent avenue and strong tree framework along the length of the Cartwright Avenue corridor.

CA.55. Tree size

Standard form trees **must** be of species capable of achieving medium- to large-scale mature size.

To ensure a strong framework for the long term, in scale with the width of corridor and adjacent building massing.

CA.56. Planting and character

Plant and tree species **must** be selected to provide a character that distinguishes from those of the neighbourhoods. The selection of plant and tree species should also reflect the varied character along Cartwright Avenue and The Common.

To reinforce a distinctive character and legibility.

CA.57. Planting diversity

Planting **must** include a diversity of types, sizes and densities.

To provide resilience and positive visual impact from day one of planting.

CA.58. Planting breaks

Planting breaks **must** facilitate the inclusion of street furniture, on-street visitor car storage and allow informal pedestrian crossing opportunities.

To ensure clear and legible public realm and viable tree planting for the long term.

CA.59. Asymmetrical planting

Planting **must** reflect the asymmetrical approach at either side of the carriageway.

To create a distinctive and consistent character to the spine corridor.



Water and drainage

CA.60. SuDS

SuDS **must** be incorporated into the public realm, and must support planting to enhance biodiversity.

To support site wide surface water drainage strategy and masterplan sustainability vision.

▶ Refer to Sitewide / Green infrastructure / Water and drainage chapter.

CA.61. Rain gardens

Rain gardens **should** be incorporated, including a drainage layer and field drain connecting to positive drainage.

To support site wide surface water drainage strategy and masterplan sustainability vision.



Movement

Streetscape

CA.62. Footways

Cartwright Avenue **must** include footways at both sides of the carriageway with a minimum width of 2 metres. Continuous crossings across accesses should be provided to ensure pedestrian priority.

To provide safe and pedestrian priority environments.

CA.63. Cycle track

Cartwright Avenue **must** include a segregated cycle track. The cycle track must connect to the existing infrastructure within Phase 1.

To provide appropriate transitional designs where uni-directionals meet bi-directionals.

CA.64. Landscape

Cartwright Avenue **must** include landscape verges at either side of the carriageway. The eastern landscape strip should have a minimum width of 2.5 metres and the western strip should expand and contract to account for accesses being formed from it and the need for vehicular storage before vehicles cross the cycle track. A flexible verge will also bring in some deflection which will reduce vehicle speeds.

To encourage healthy, low-speed streets.



CA.65. Street widths

Street widths **should** be as narrow as practical, please see illustrative section below for reference. Widths must be informed by operational requirements for emergency and waste services.

To encourage healthy, low-speed neighbourhoods.

CA.66. Carriageway

The road carriageway width **must** be as narrow as practical, allowing sufficient space for all forms of travel including buses.

To encourage healthy, low-speed neighbourhoods.

CA.67. Defensible space

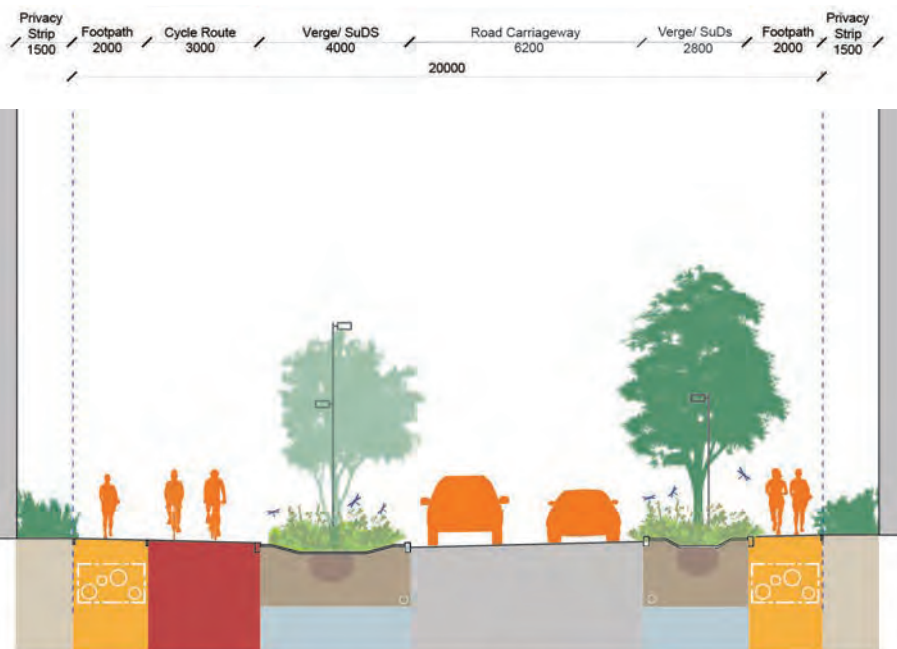
Cartwright Avenue **must** include defensible space strips in front of ground floor habitable rooms.

To provide privacy to living spaces.

CA.68. Crossings

Cartwright Avenue **must** include both formal and informal points for pedestrian and cycle crossings.

To provide safe and pedestrian priority environments.



The Common

CA.69. Visitor car storage

Visitor car storage on landscape strips **must** be limited to safeguard the predominant landscape function along Cartwright Avenue.

To promote active travel and green streets.

CA.70. Cycle crossings

The alignment of the carriageway and landscape strips **must** allow sufficient space for cyclists to wait to cross Cartwright Avenue without compromising the cycle and pedestrian movements along it. Crossings and cycling standing areas should align with breaks in massing along Cartwright Avenue; to facilitate cycling and pedestrian movement from Neighbourhoods to Cartwright Avenue.

To promote active travel.

CA.71. Adoptable standards

The material palette and details **should** be designed to adoptable standards.

To ensure the standard of public space is consistent with wider Cambridge.

CA.72. Connected cycle tracks

The Common **must** extend and connect the two cycling routes: Turing Way/Cartwright Avenue to the west and The Ridgeway to the east. This connection must suitably address level differences.

To provide a legible, continuous network of accessible routes.

CA.73. Accessibility and inclusivity

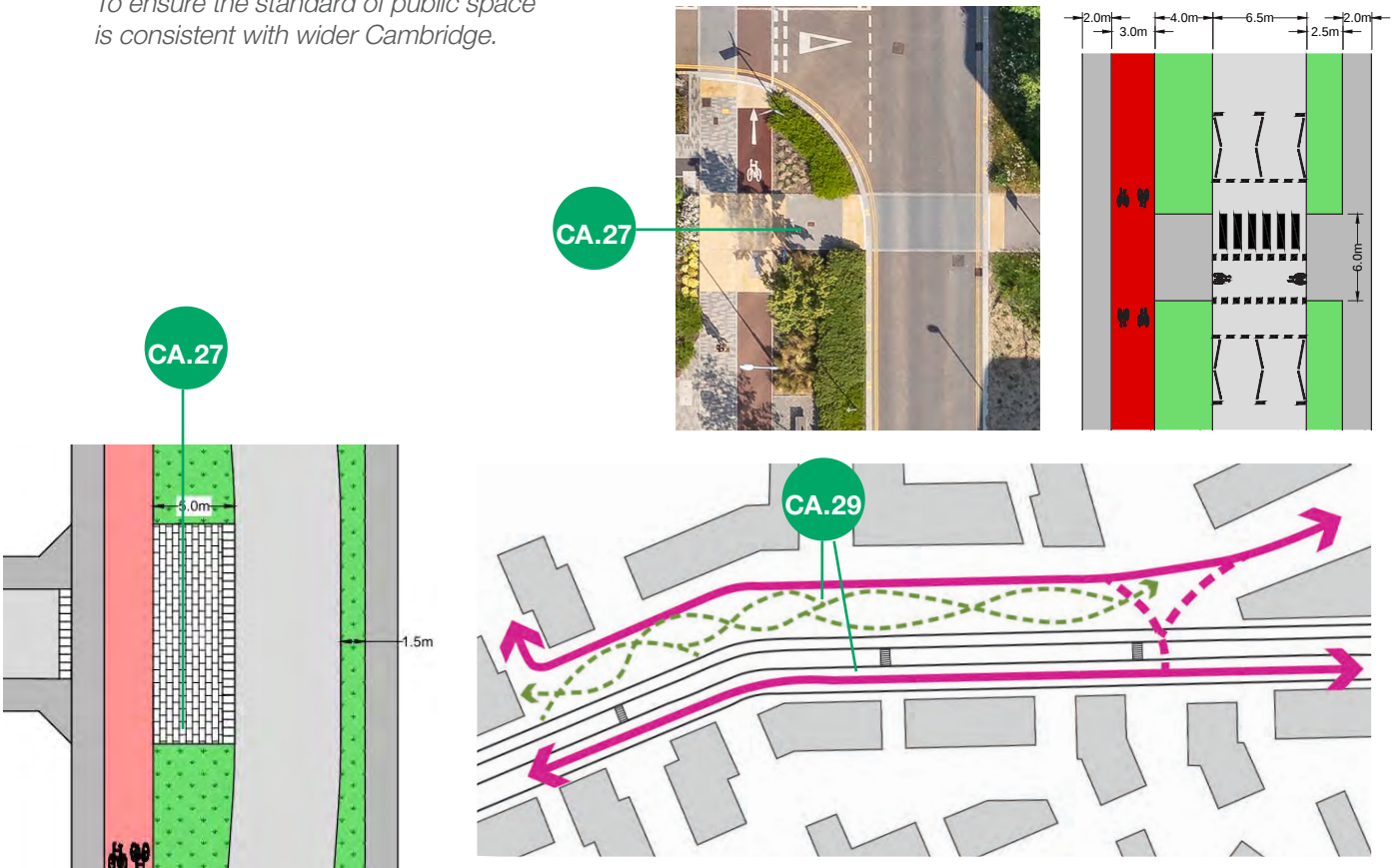
Routes **must** provide access no steeper than 1:21 gradient with compliant level landings.

To provide exemplar public realm serving the whole community.

CA.74. Mobility hub

The design of The Common **should** include a mobility hub, where access to a variety of transport modes can be achieved along with buses.

To promote active and sustainable travel.



Built Form

Layout

CA.75. Building breaks

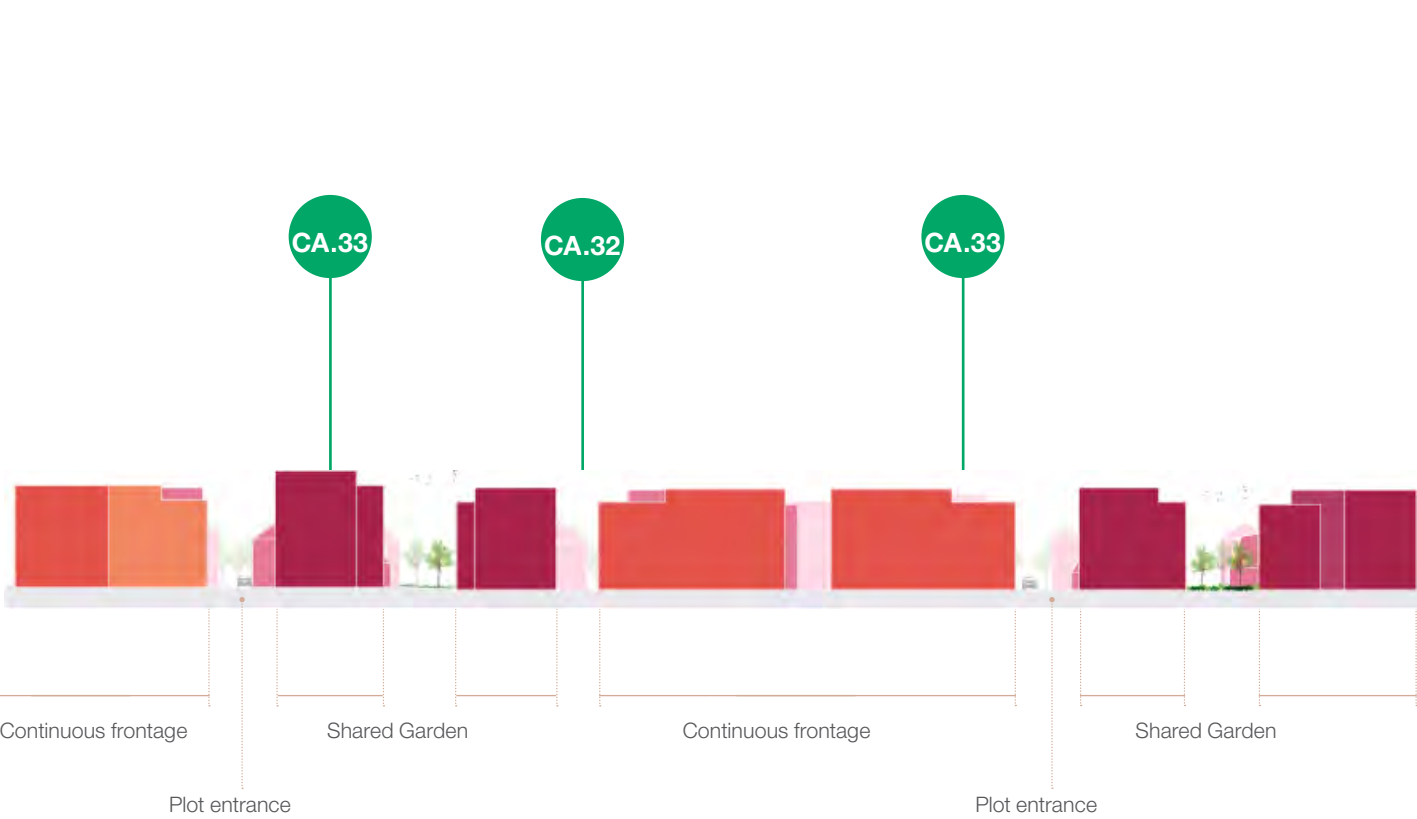
Where breaks within the streetscape are proposed, breaks **must** indicate an entrance to a neighbourhood or a key view through to Brook Leys.

To provide legibility to neighbourhoods along Cartwright Avenue.

CA.76. Buildings marking Shared Gardens

Where the Shared Gardens occur, buildings **should** act as a marker to the gardens. This can be achieved by rotating buildings so a smaller facade faces Cartwright Avenue.

To express entrances to Shared Gardens along Cartwright Avenue.



Massing and character

CA.77. Location of height

Taller buildings **should** be positioned in places that assist wayfinding, for example at the book ends of The Common.

To contribute to the legibility of the massing.

CA.79. Building tops

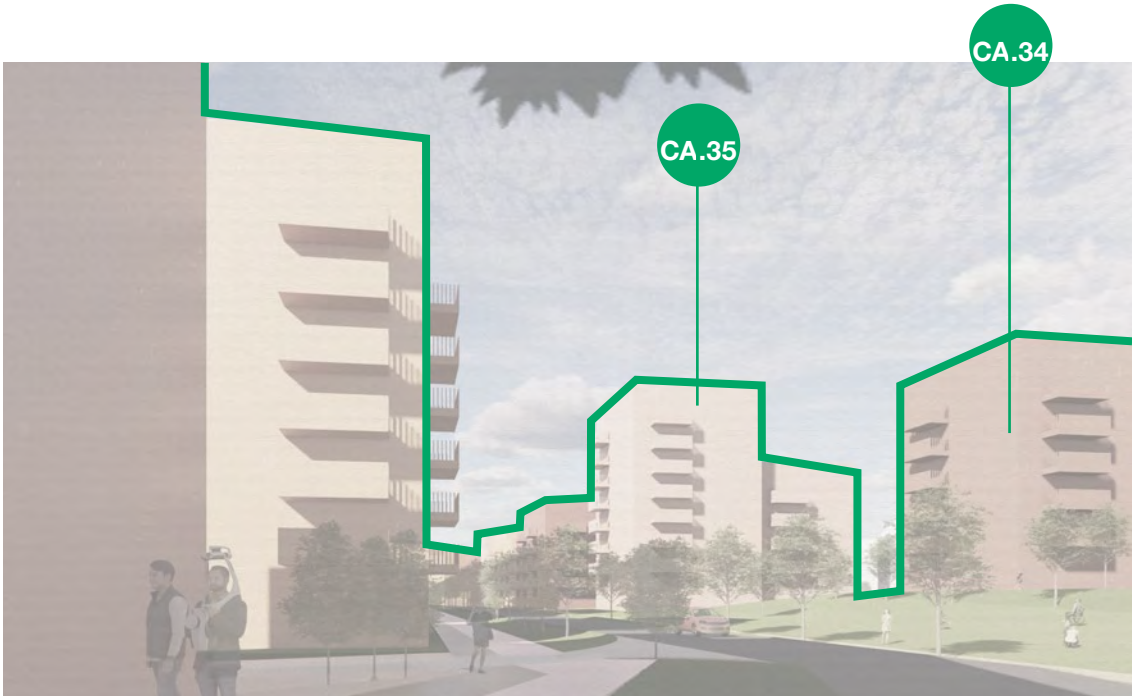
Where the building top is articulated it **should** materially read as one building. For example, set backs are permitted but should continue the material language of the building below.

To ensure buildings have strong vertical emphasis with a simplicity of form.

CA.78. Taller buildings

Taller buildings - 7 storeys and over- **must** be designed for 360 degree visibility.

To ensure buildings of height read from long distances.



Innovation Street

Innovation Street



Innovation Street will be a vibrant makers quarter that reimagines the standard industrial estate and more traditional office campus through a street-based urbanism approach that prioritises people, and well-defined public spaces that support sociability and collaboration. It will provide flexible mid-tech and employment space within dynamic built forms, challenging standard big box typologies in favour of articulated massing, varied rooflines and a human scale grain that feels rooted in Cambridge. The place will prioritise active frontages, walkability and integration with the wider neighbourhood.

The area will support a diversity in scale to attract a variety of tenants and foster a vibrant community. Buildings and spaces will express innovation not just in use, but in built form and placemaking.



Green and Blue Infrastructure

Character and levels

IS.01. Build from existing assets

Landscape design **must** build from the existing assets such as the hedgerow and trees along the Public Right of Way or the trees and plants along Huntingdon Road.

To retain and protect the site's natural assets and biodiversity value.

IS.02. A natural edge

The landscape design to the west of development **must** contribute to define a natural setting which takes it reference from the landscape character of Brook Leys.

To provide a distinctive character in keeping with the landscape setting.

IS.03. A wooded edge

To the northern site boundary, a strongly wooded character with trees capable of reaching large scale mature stature **must** be formed.

To provide a strong green framework and soften view into the site from the north.

IS.04. A green gateway

Along the Huntingdon Road boundary, the existing tree and mature hedgerow line **must** be retained and strengthened with new native species tree and understorey planting.

To enhance existing character, and create a strong green frontage to Huntingdon Road.

IS.05. Landscape structures

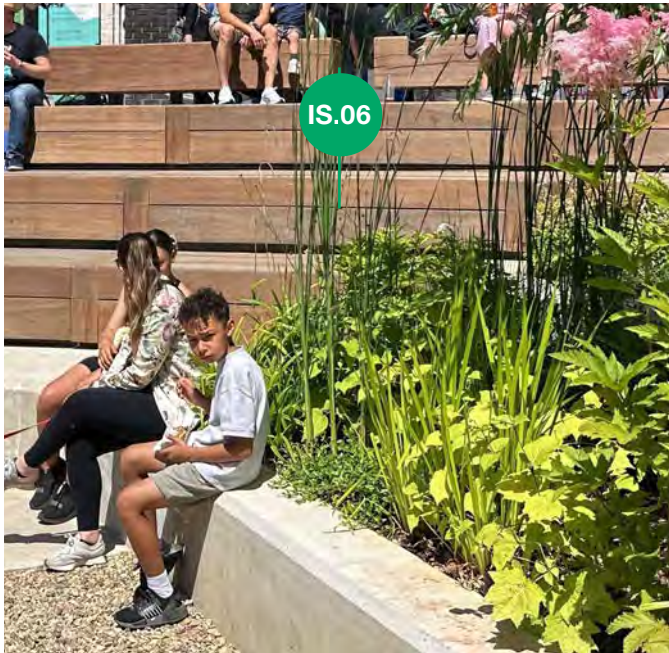
Landscape design **must** respond to the specific character of each area of IS and contribute to its definition. For example through more formal street trees along the central street and informal landscape character to the west (see IS.02).

To support placemaking and create a legible public realm.

IS.06. Managing level changes

Where level changes are necessary, these **should** be integral to the landscape design, for example edges also form seating.

To support efficient use of space, and create a coherent and cohesive public realm design.



Community and amenity

IS.07. Space to congregate

The landscape design **must** include spaces for gathering such as seating areas - next to building entrances - or common space to have lunch outdoors.

To promote social interaction and foster community cohesion.

IS.08. Space for small informal events

The design of the public realm **must** include an area of hard standing with suitable space for ad-hoc pop-up events. This should have visual connection with Cartwright Avenue. It should include spaces for temporary car storage, power connection and be integrated within the landscape proposals.

To provide a well integrated flexible use public realm space.

IS.09. Mix use northern activity hub

Landscape design **must** contribute to the emergence of a northern hub. This could be achieved, for example through visual, pavement and / or planting connections between the different uses of the hub, such as a mobility hub next to the sports facilities, nursery or a cafe, informal events or lunch area at the pedestrian access to the northern employment area along Cartwright Avenue.

To create a cohesive public realm which clearly defines the northern hub as a distinctive place.



Planting and biodiversity

IS.10. Existing habitats

Existing habitats of high biodiversity value such as the hedgerow adjacent the Public Right Of Way **must** be protected, retained and enhanced.

To retain and protect the site's natural assets and biodiversity value.

IS.11. Planting on Innovation Street

Innovation Street - the street at the centre of the Place- **must** include landscape strips at either side of the carriageway. Landscape design should accommodate cycle crossings.

To maximise soft landscape in the public realm and support active travel objectives.

IS.12. Planting along Huntingdon Road

The existing trees and planting along Huntingdon Road **must** be retained and enhanced to filter the views to sensitive receptors.

To provide an appropriate response to the surrounding context.

IS.13. Planting adjacent to the A14

The size and density of the planting adjacent to the A14 **must** filter the views to the site. Planting and buildings must make a positive contribution to the character of the wider area.

To filter the views from the north of the site and create a positive environment.

▶ Refer to Sitewide / Built form / Massing and character.

Water and drainage

IS.14. Planting within car storage

Car storage zones **must** include trees and/or planting in between car storage spaces and at the edges.

To provide visual breaks on groups or lines of parking, and create comfort in the public realm.

IS.15. SuDS

SuDS **must** be incorporated into the public realm and landscape framework, and must support planting as part of the surface water drainage system. Where there is sufficient space, swales can be used.

To enhance biodiversity and support the masterplan sustainability objectives.

▶ Refer to Sitewide / Green infrastructure / Water and drainage.



Movement

IS.16. Cycling and pedestrian extension of Cartwright Avenue

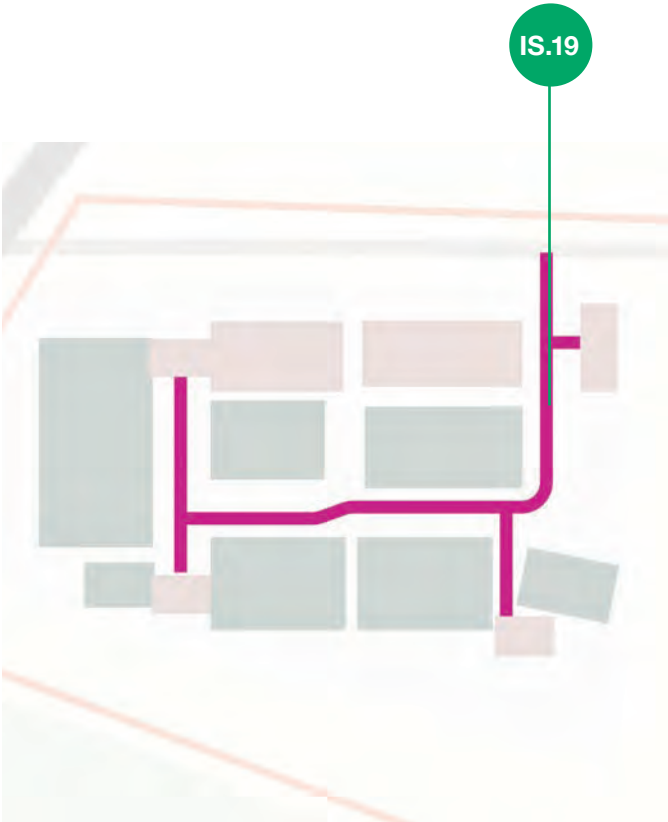
The segregated cycle track and south-western footpath on Cartwright Avenue **must** be extended to provide access to the northern development Zone.

To encourage active travel and healthy lifestyles.

IS.17. Street components

Innovation Street - the street at the centre of the Place- **must** include a carriageway, landscape strips at both sides, at least one footpath or shared user path to the western side. This central street should read as human scale with buildings at the edges and planting contributing to this.

To create a streetscape that puts people before cars.



IS.18. Vehicular access

Vehicular traffic **should** have access from Huntingdon Road with an opportunity for an alternative access to the development zone from Cartwright Avenue.

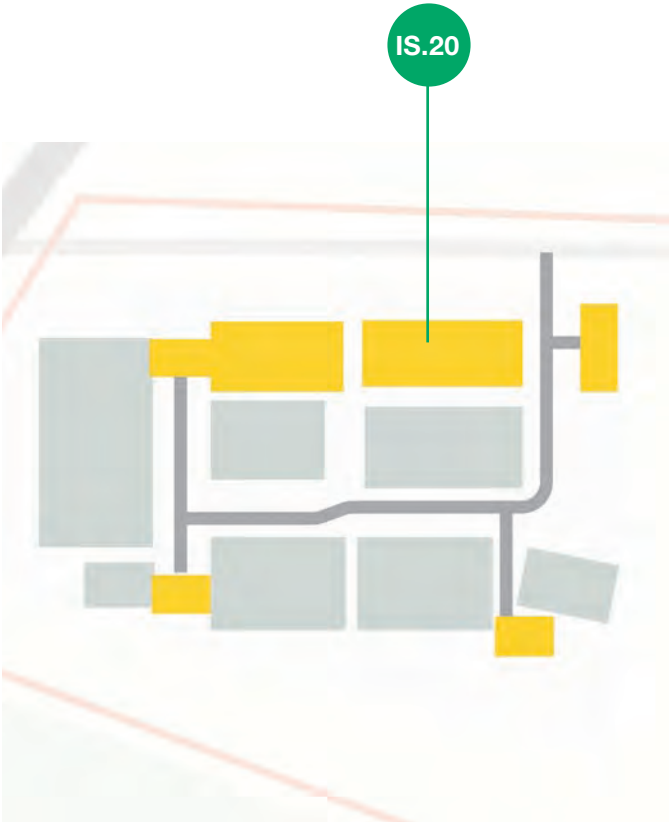
To ensure larger vehicle traffic doesn't conflict with residential areas.

▶ Refer to PP3-10003-Access and Movement Parameter Plan

IS.19. Vehicular movement northern development zone

Vehicular movement including servicing **must** be organised around a central street.

To consolidate vehicle movement and promote car-free areas of the site.



IS.20. Car storage

Car storage **should** be consolidated in areas where it is less visible, such as in the space between Huntingdon Road and the back of buildings, or next to service yards. They should be designed with planting to soften the car storage runs.

To reduce visibility to cars and promote greening.

IS.21. Cycle storage

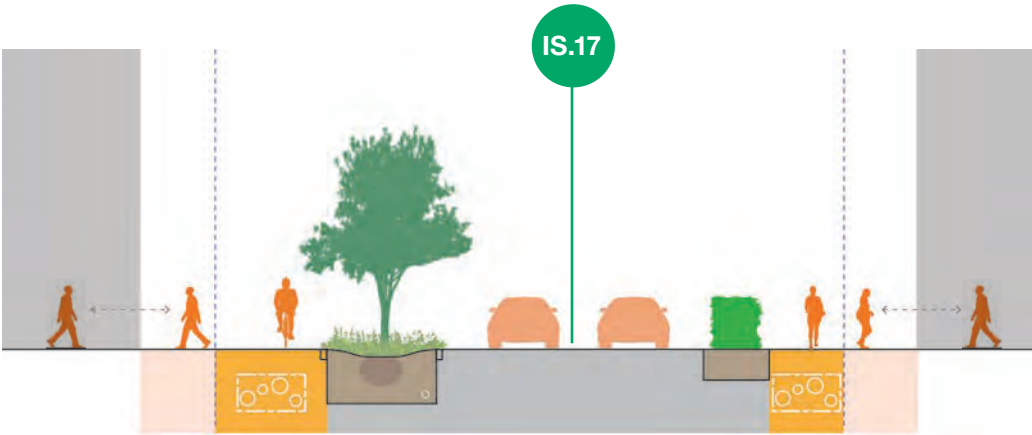
Cycle storage **must** be provided both externally and internally, to encourage active travel. Commercial cycle storage should be split between long stay and short stay. Long stay cycle storage should be safe secure and covered. Short stay cycle storage must be convenient and well designed as part of the public realm.

To encourage active travel and healthy lifestyles.

IS.22. Location and consolidation of service yards

Vehicle access routes must be minimised and service yards must be located to the back of buildings, away from the central street, and must not dominate frontages or key spaces. Where possible, service yards should be consolidated and shared between buildings to improve efficiency and reduce land take.

To keep street frontage active and free of vehicles.



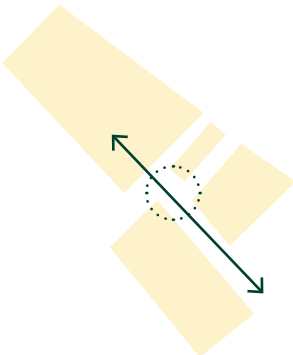
Built Form

Layout

IS.23. Northern Hub connecting Innovation Street and Cartwright venue

A hub **should** be located at the northern end of Cartwright Avenue, bringing together the sports area, mobility hub, and northern plot gateway, to create a focal space. This could be achieved, for example through creating an area of visual interest through vistas or architectural detail, and / or through clustering shared facilities or ancillary uses.

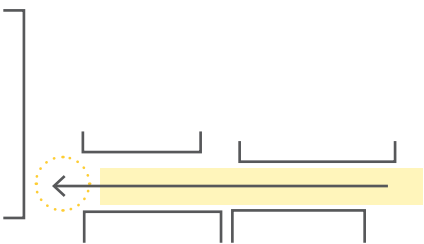
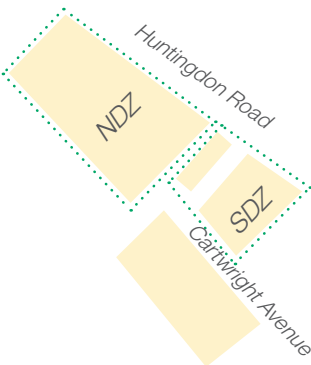
To provide space for community and foster social interaction.



IS.24. Buildings defining streets

Buildings **must** be laid out and aligned in ways that form well defined streets. On the southern development zones (SDZ), buildings must define the edge of Cartwright Avenue. In the northern development zone (NDZ) buildings define a compact internal street that is visually connected to Cartwright Avenue.

To create a street based development with positive frontages.



IS.25. Gateway Definition

Buildings framing Cartwright Avenue **must** clearly define the entrance into the wider neighbourhood. This should be achieved through strong frontage definition, articulated building forms, façade hierarchy, and active ground floor uses. Building design must be exemplary and should signal arrival and contribute to a strong sense of place.

To create clearly defined gateways into development zones.

IS.27. Entrances

Building entrances **must** be clearly visible and accessed directly from the street with reception and office spaces located at the front of the building to animate the public realm and create an active edge. Street facing façades should create a good degree of transparency allowing views into active internal areas. Back of house functions, including service access, plant room and storage must be located away from key frontages and screened from public view. Building layouts must establish clear fronts and backs.

To provide positive frontages and active streets.

IS.26. Frontages

Buildings **must** be laid out and arranged to form well defined streets and spaces, with strong frontages that contribute to an active public realm. In the SDZ, buildings must clearly define the edge of Cartwright Avenue. In the NDZ, buildings must define a compact internal street that establishes a clear visual connection to Cartwright Avenue.

To provide positive frontages and active streets.

IS.28. Ancillary buildings

Should they be required, ancillary buildings, such as substations or plant rooms **must** be located away from the central street, next to service yards.

To remove street clutter and screen ancillary buildings.



Massing and character

IS.29. Simple massing

Buildings **must** be simple in form and have variety in scale whilst avoiding stepping along the streetscape.

To ensure a consistent elevation reinforcing the street, whilst bringing variety of scale.

IS.31. Facade and material treatment

Building façades **must** be simple and contribute to the character of the area. This could be achieved for example through tone and materiality, clear hierarchies, windows and openings and/or other architectural detail.

To integrate large building forms into the landscaped setting.



Refer to Sitewide / Built Form / Massing and character.

IS.33. A human scaled streetscape

Building form and façade articulation **must** establish a strong sense of human scale along street frontages. This could be achieved by breaking down the massing with architectural expression, setbacks and or roof articulation as well as with details around windows or entrances.

To reinforce the street with a sense of human scale.

IS.35. Building setting

If required to mitigate the visual impact of massing in sensitive views, buildings should be set back an appropriate distance from Huntingdon Road

To contribute to the legibility and character of the wider area.

IS.30. Roofscape articulation

Building form **must** reflect the use of the buildings and contribute to an articulated skyline. This could be achieved, for example through articulated roof forms.

To ensure a varied roofscape and dynamic silhouette.

IS.32. Positive environment

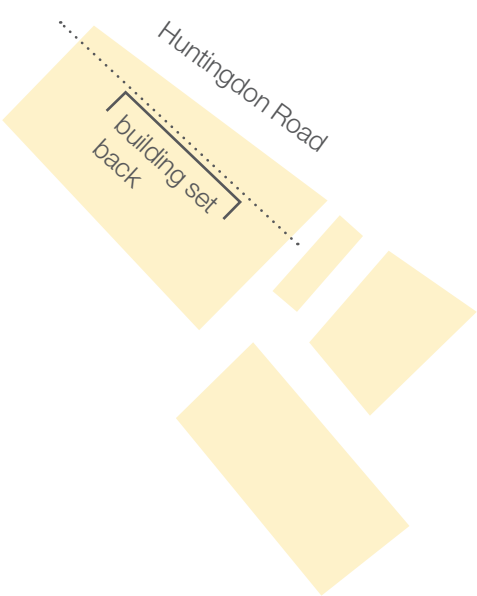
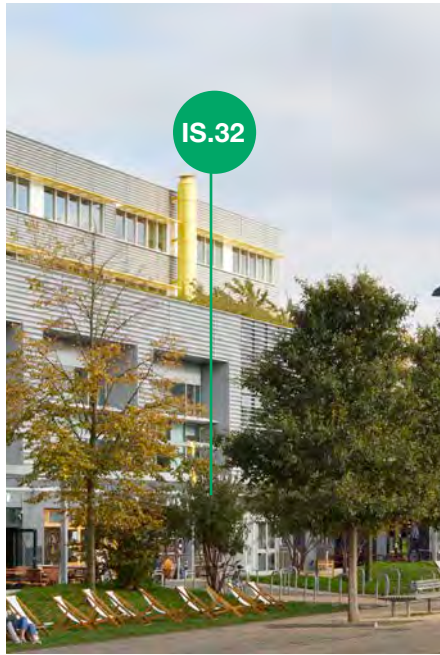
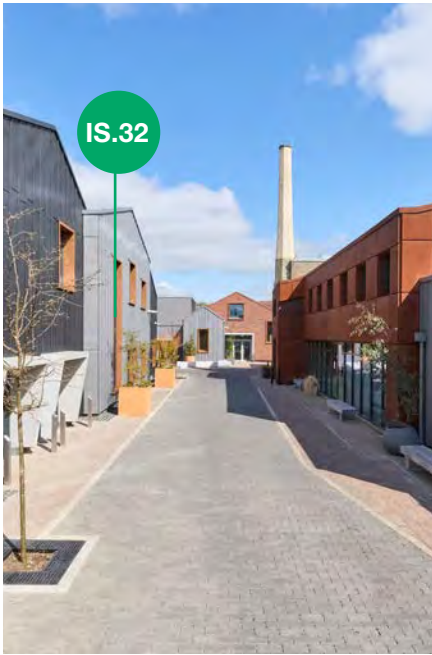
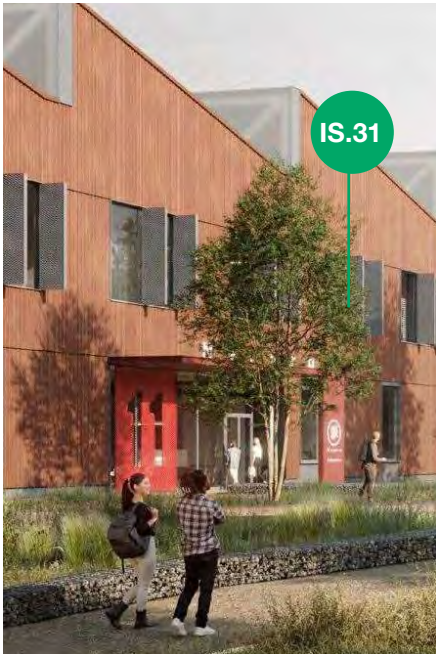
All building façades, including back façades, **must** be designed to make a positive contribution to the streetscape, especially when they are exposed to a number of views - such as north or west of the site.

To contribute to the character of the wider area.

IS.34. Roof plant

Roof plant **must** be screened or integrated to the design of the rest of the façades.

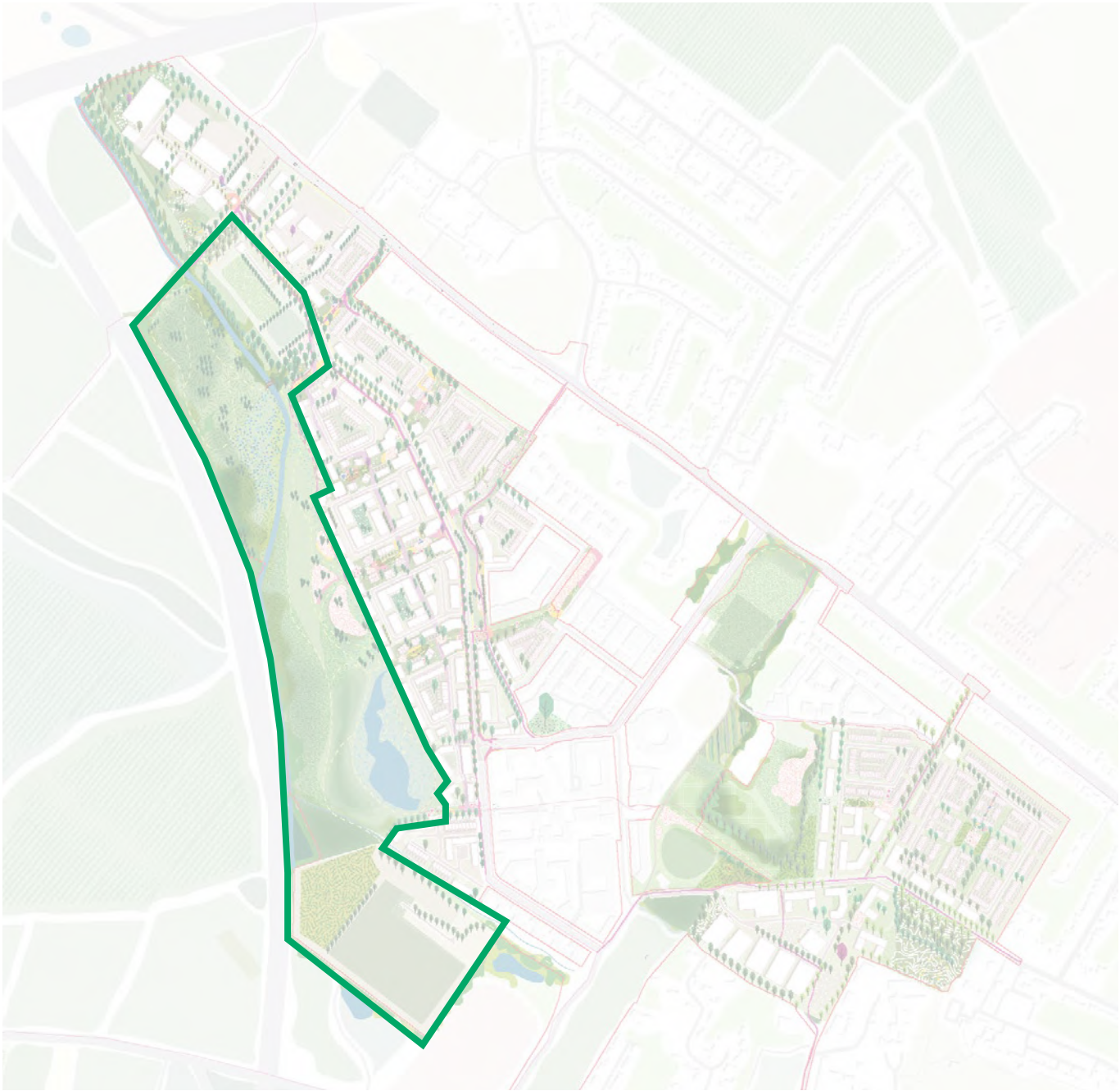
To avoid visible plant areas from the streetscape.



Indicative set back, distance to be determined at defined at reserved matters stage

Brook Leys

Brook Leys



Brook Leys is the key, large scale green space for residents of Eddington, forms part of the Cambridge greenbelt and forms a buffer between the M11 and Eddington, helping screen views, mitigate noise and provide habitats and green infrastructure. Existing features such as Washpit Brook and the phase 1 lagoon will be integrated into a new landscape for Brook Leys. New features will include extensive tree planting , pedestrian and cycle routes, SuDs features, play, sports and community gardens.



Green and Blue Infrastructure

Character and levels

BL.01. Coherent character

Material and furniture selection, including play equipment **must** be coordinated throughout the Brook Leys, with a preference for natural materials.

To create a consistent and coherent materials palette in keeping with it's natural setting.

BL.02. Publicly accessible

Brook Leys **must** be publicly accessible, except for the areas dedicated to nature.

To promote contact with nature and support the site wide amenity strategy.

BL.03. Nature-based solutions

Level changes from the adjacent development edge **must** be achieved using natural solutions, of no greater than 1:3 gradient. This could include land profiling or gabion baskets.

To facilitate the usability of the public realm and reduce the carbon footprint.

BL.04. Gentle undulation

Landforms **must** be sited and graded sympathetically, to create gently undulating forms in keeping with the local landscape topography.

To respect local landscape character.

BL.05. Landform at north west corner

Surplus material from other parts of the development **should** be located at the north west corner of Brook Leys.

To reduce the carbon footprint.

BL.06. Inclusive access from Shared Gardens

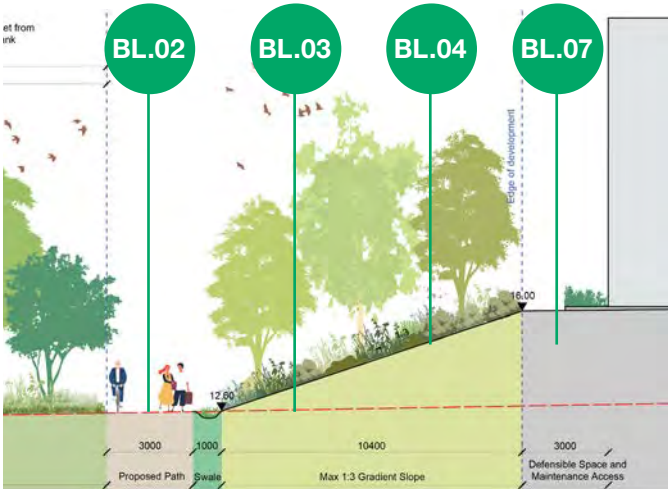
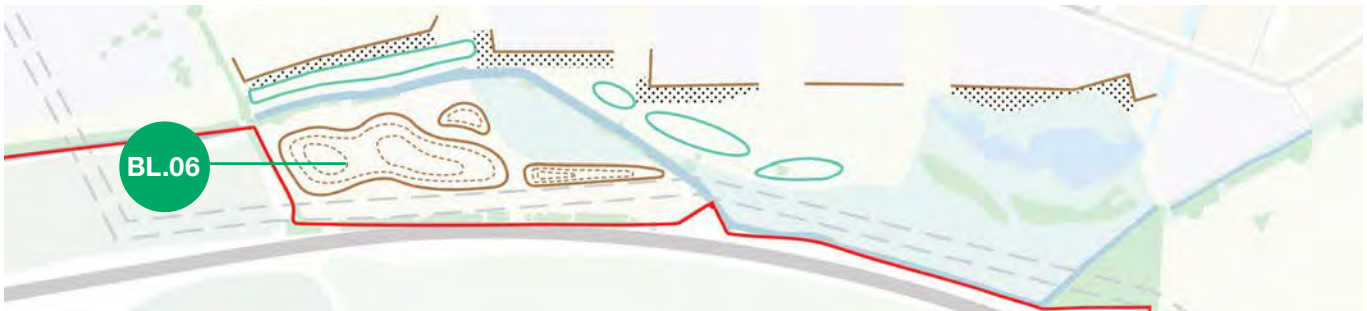
Access from each shared garden **must** be inclusive.

To provide continuity of access for all and permeability of the public realm.

BL.07. Access from homes

Individual homes fronting Brook Leys **should** include direct and inclusive access.

To activate the development edge and facilitate easy access to the site wide amenity strategy.



Community and amenity

BL.08. Informal recreation

Brook Leys **must** incorporate informal recreation including cycle links. Walking routes must be included and must connect to each shared garden, the existing Phase 1 footpath network, and existing Public Rights of Way (PROW).

To support the site wide amenity strategy.

BL.09. Distribution of amenity

Amenity and usable areas **must** be located to the east of Brook Leys; to leave a nature-only area to the west of Brook Leys.

To balance the dual aims of providing public amenity, and enhancing biodiversity.

BL.10. Nature Play

Playable spaces **must** include nature-based solutions such as timber or landforms.

To encourage contact with nature.

Refer to Sitewide / Green and Blue Infrastructure / Community and Amenity

BL.11. Equipped play

Brook Leys **must** include equipped local playable spaces;

To support the strategic placement and quantum of play across the site.

BL.12. Public Art

The design of the public realm **should** include public art, suggesting spaces for people to meet and where practical providing space for it.

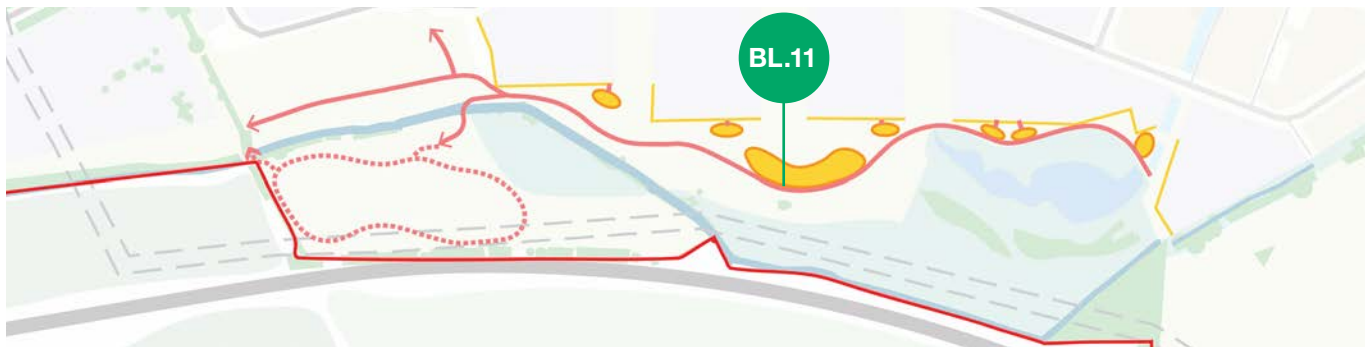
To encourage social interaction.

Refer to Public art strategy

BL.13. Signage

Brook Leys **must** include a coordinated signage strategy

To assist with wayfinding.



Planting and biodiversity

BL.14. Naturalistic character

Planting **must** contribute to a naturalistic character in keeping with the local landscape character to form the urban fringe.

To support the masterplan vision of a soft transition from rural to urban character,

BL.15. Habitat creation

Brook Leys **must** maximise the potential for habitat creation;

To support biodiversity strategies across the site.

BL.16. Distribution of natural habitats

Natural habitats **must** be located to the west of Brook Leys, with restricted access to the west;

To balance the dual aims of providing public amenity, and enhancing biodiversity.

BL.17. Tree and woodland planting

The design of Brook Leys **must** include tree and woodland planting;

To support biodiversity strategies

BL.18. Location of trees

Tree and woodland planting **must** be located to create visual breaks and layering to the development edge.

To support the masterplan vision of a soft transition from rural to urban character and respond to visual impact assessment studies.

BL.19. Landmark trees

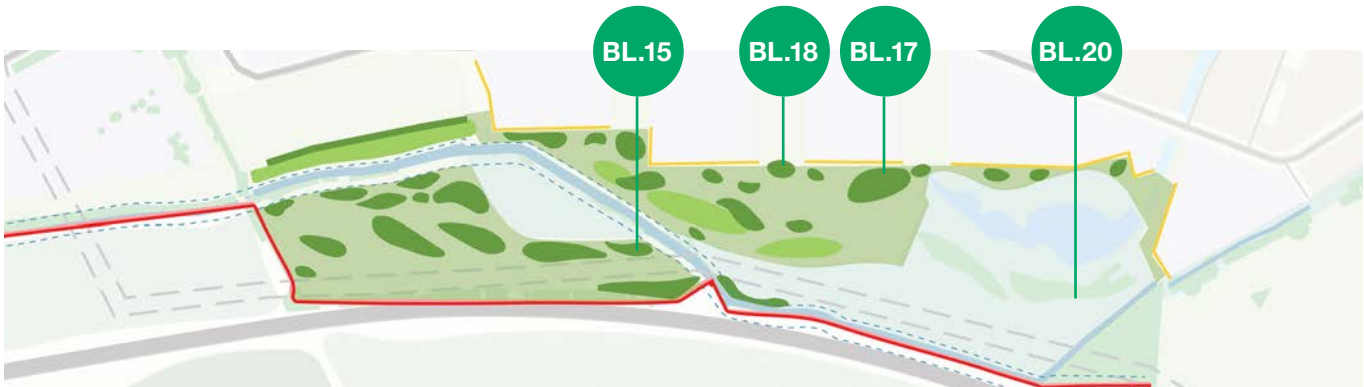
The design of the public realm **must** include landmark trees.

To provide mature canopy height and cover for the long term.

BL.20. Legibility of landscape strategies

A landmark tree or group of medium to large scale trees **must** be located at the junction of each Shared Garden; to mark its position reinforcing wider landscape strategies.

To provide mature canopy height and cover for the long term and support site wide legibility



Water and drainage

BL.21. Detention basins

Brook Leys **must** incorporate detention basins sized to accommodate the volume requirements of the site wide surface water drainage strategy.

To support the site wide surface water drainage strategy.



Movement

BL.22. Connectivity

The design of Brook Leys **must** include shared user paths connecting Phase 1 with the northern part of the site and the development zones and shared gardens.

To promote active travel and contact with nature.

BL.24. Running

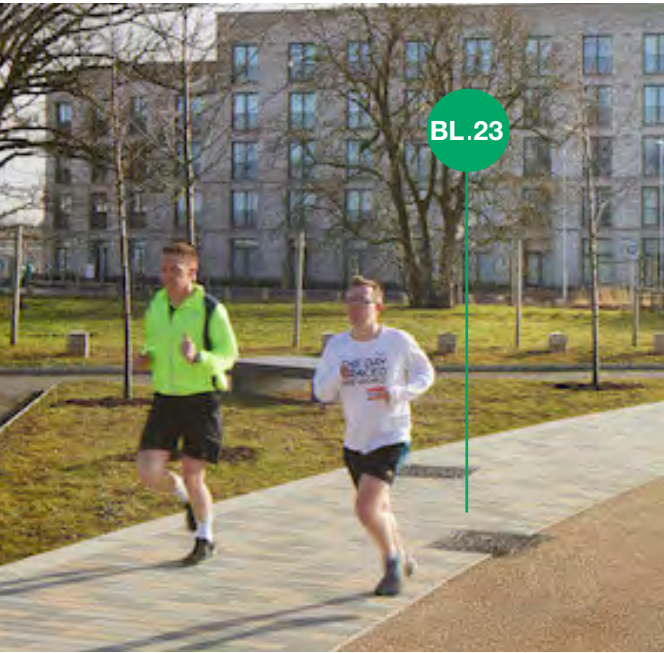
Routes **should** be designed with widths that facilitate running alongside other forms of active movement and connect to other routes that all together form running circuits, such as 5 kilometres routes.

To promote active and healthy lifestyles.

BL.23. Active trail

The design of Brook Leys **must** provide a shared user path active trail. This trail should prioritise nature and provide visual connections to encourage learning about biodiversity.

To promote active travel and contact with nature.



Built Form

Layout

BL.25. Private amenity and thresholds

The location and treatment of the private amenity **must** contribute to the activation of the public realm. This could be achieved, for example through openings, low and/or visually permeable thresholds of a maximum of 1 metre), and/or the inclusion of benches or seating. Private amenity could also be located at grade or slightly elevated from the public realm.

To soften the edge of development to Brook Leys



Massing and character

BL.26. Massing breaks hierarchy

Massing breaks that flank entrances to Shared Gardens **must** be wider than other gaps. See diagram on this page for an illustrative example.

To assist with the legibility of the landscape strategy.

BL.28. Integration of smaller buildings

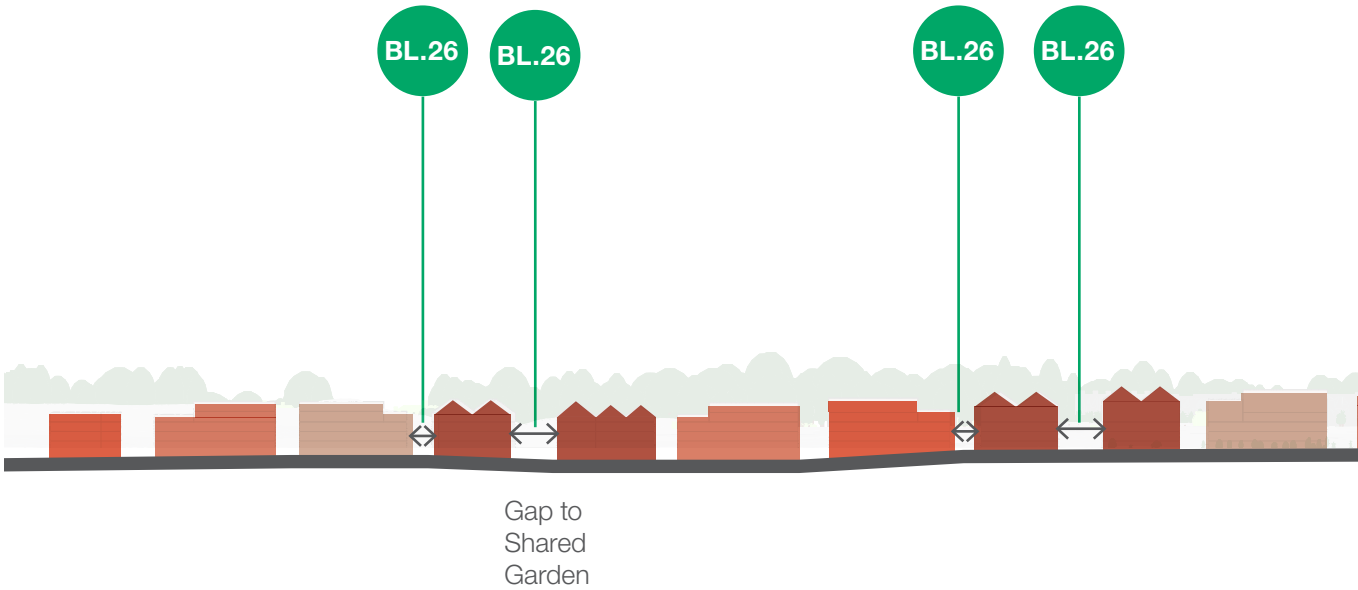
Where required, small buildings, such as pavilions, infrastructure, or others **must** be well integrated with the landscape, and have a scale that is second in hierarchy to landscape design and features.

To safeguard the nature-based character of Brook Leys.

BL.27. Massing breaks width

Massing breaks width **must** be as narrow as possible; to mitigate the noise from the M11.

To assist with the legibility of the landscape strategy and provide a continuous edge to the city.



Appendix

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Shared Gardens

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Neighbourhoods

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Community Lane

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Gravel Hill

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Cartwright Avenue and The Common

- p. 153 Courtesy of YTL

Innovation Street

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Brook Leys

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